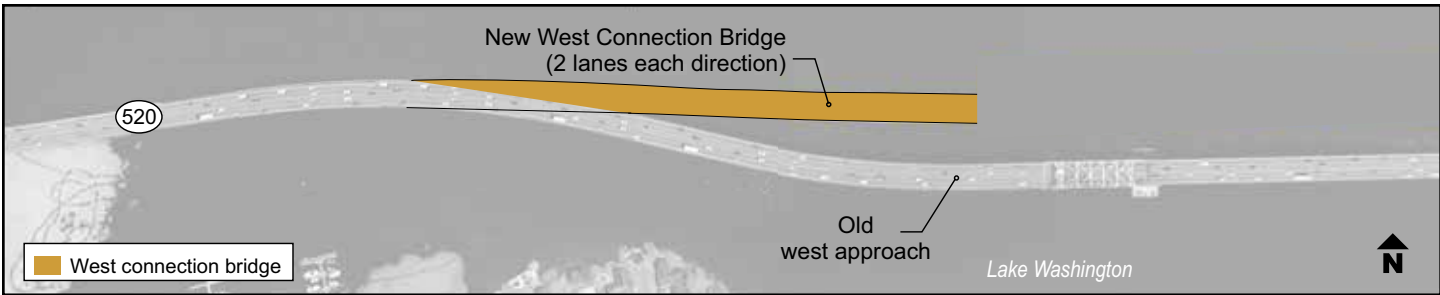


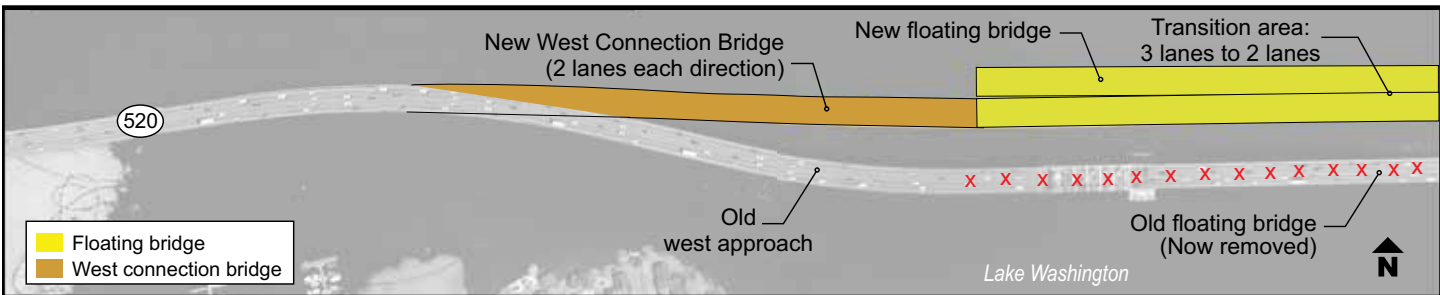
How we will connect the new SR 520 bridges

Construction is taking place in the following phases:

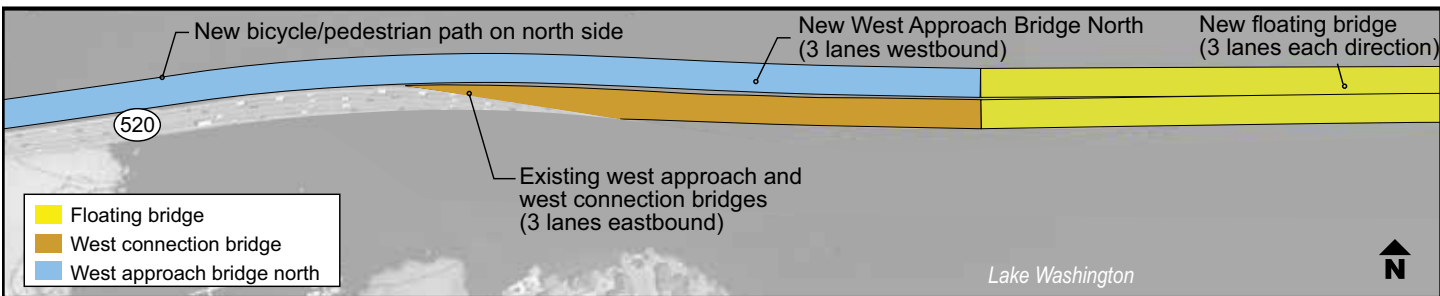
**Step 1 - West Connection Bridge:** In fall 2014, we completed an interim bridge designed to connect the new floating bridge to the existing west approach bridge.



**Step 2 - Floating bridge:** The new floating bridge on Lake Washington opened to traffic in April 2016.



**Step 3 - West Approach Bridge North:** In fall 2014, we started work on the West Approach Bridge North. When WABN opens to traffic in summer 2017, westbound traffic will travel on the new structure while eastbound traffic will travel on the existing west approach.



For more information:

**Visit:** [www.wsdot.wa.gov/projects/sr520bridge](http://www.wsdot.wa.gov/projects/sr520bridge)

**E-mail:** [sr520bridge@wsdot.wa.gov](mailto:sr520bridge@wsdot.wa.gov)

**Program info line:** 206-770-3554  
(program info/staffed 8 a.m. - 5 p.m., Mon-Fri)

**Construction hot line:** 206-708-4657  
(24-hr hot line/for urgent issues)



**Americans with Disabilities Act (ADA) Information:** This material can be made available in an alternate format by emailing the Office of Equal Opportunity at [wsdotada@wsdot.wa.gov](mailto:wsdotada@wsdot.wa.gov) or by calling toll free, 855-362-4ADA(4232). Persons who are deaf or hard of hearing may make a request by calling the Washington State Relay at 711.

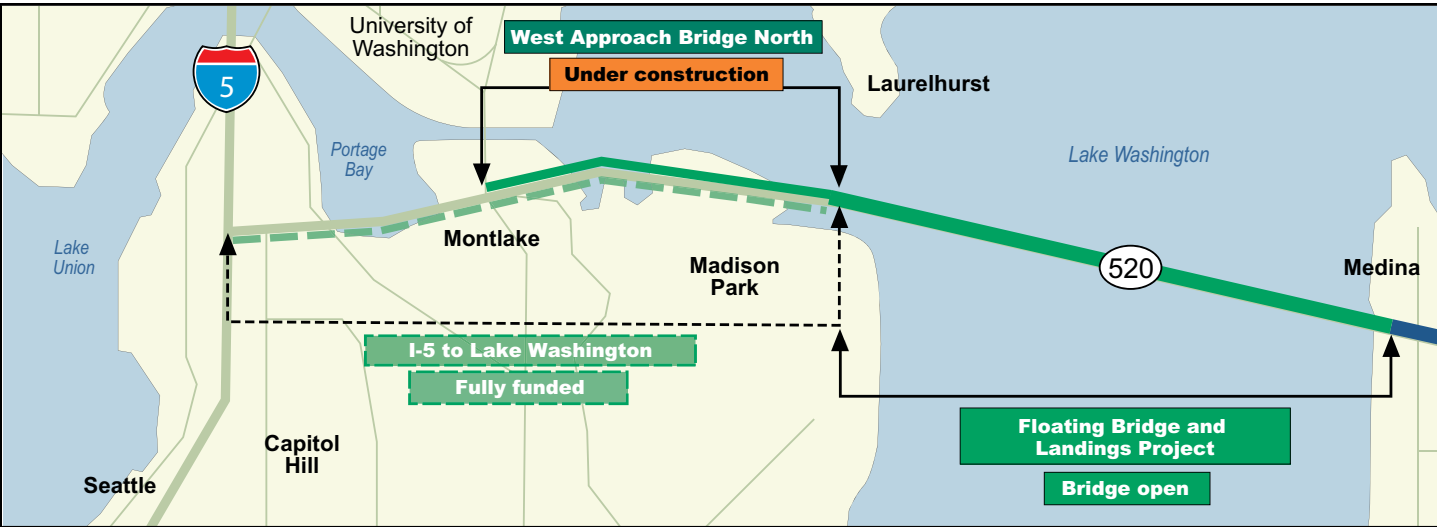
**Title VI Notice to Public:** It is the Washington State Department of Transportation's (WSDOT) policy to assure that no person shall, on the grounds of race, color, national origin or sex, as provided by Title VI of the Civil Rights Act of 1964, be excluded from participation in, be denied the benefits of, or be otherwise discriminated against under any of its federally funded programs and activities. Any person who believes his/her Title VI protection has been violated, may file a complaint with WSDOT's Office of Equal Opportunity (OEO). For additional information regarding Title VI complaint procedures and/or information regarding our non-discrimination obligations, please contact OEO's Title VI Coordinator at (360) 705-7082.

SR 520 BRIDGE REPLACEMENT AND HOV PROGRAM



Building the West Approach Bridge North

New structure will carry westbound traffic from new floating bridge to Seattle



The West Approach Bridge North (WABN) is the latest phase of SR 520 construction. This three-lane, 1.2-mile-long structure, built to modern earthquake standards, will connect westbound travelers from the new floating bridge to the Montlake vicinity in Seattle (see additional details on back page).

The project also will connect the new, 14-foot-wide bicycle/pedestrian path from the Eastside and new floating bridge to Montlake, expanding travel options to local and regional destinations. In addition, this project extends to Seattle a new transit/HOV lane to support bus and carpool reliability.

Environmental mitigation and community enhancements from this project will improve the Arboretum, create a new local park along Portage Bay, and enhance local natural resources.

This phase of work was fully funded after the Washington State Department of Transportation received a \$300 million federal loan. Construction began in fall 2014, and the new bridge is expected to open to drivers in summer 2017.



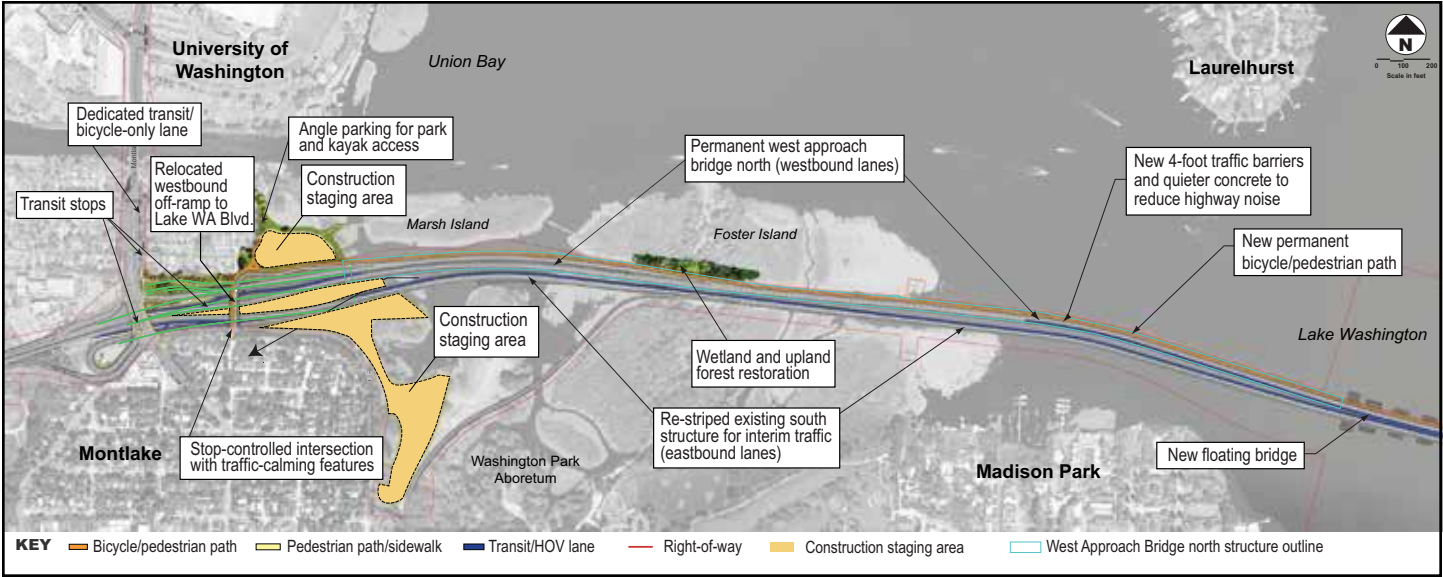
Photo of damage to a hollow bridge column supporting the existing west approach bridge. The column was damaged – and later repaired – after a barge collision in 2000.



All new WABN columns are solid concrete strengthened with rebar. WSDOT worked with the Seattle Design Commission to incorporate fluted columns into the WABN design for aesthetic appeal.



Key features of the West Approach Bridge North



Why WSDOT is building the new west approach now

WSDOT is making improvements to the SR 520 corridor in phases as funding becomes available. Replacement of vulnerable structures, including WABN, is the first priority. This phased implementation, outlined in the construction program’s 2011 Final Environmental Impact Statement, is common for projects of this scale.

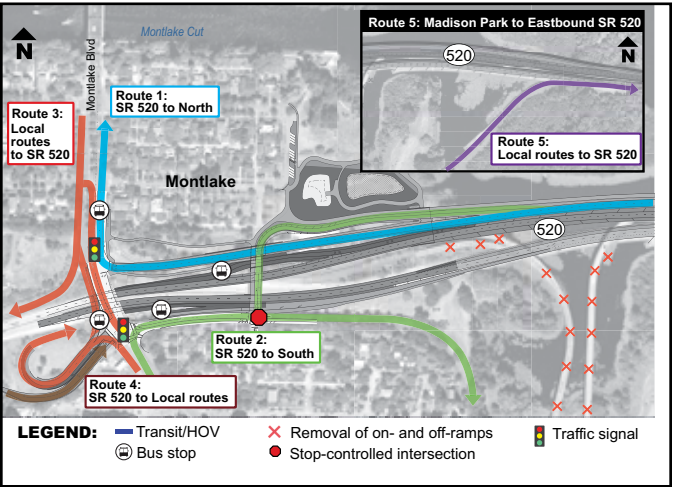
The WABN project will bring a number of key benefits, including:

- Moving traffic onto a new, seismically safer structure that can accomodate four lanes if the existing approach bridge collapsed.
- Connecting the new cross-lake regional bicycle and pedestrian path to the Montlake vicinity.
- Connecting the new transit/HOV lanes across Lake Washington to multimodal hubs in the Montlake and University of Washington areas.
- Implementing noise-reduction strategies such as 4-foot traffic barriers, quieter bridge joints and quieter concrete to reduce highway noise.

With funding now secured for SR 520’s remaining west side improvements in Seattle, WSDOT will continue working with the city of Seattle and other stakeholders to finish the design of those planned elements and complete the corridor’s reconstruction all the way to I-5.

Will the new bridge affect traffic in the Montlake area?

This bridge will not significantly increase overall traffic volumes in the Montlake area. Today, traffic volumes are lower in the Montlake area because of tolling on SR 520. We expect to see similar volumes after WABN opens. Local travel times will be approximately the same after this bridge is operational as they are today. When construction is complete, drivers will use similar routes to travel to their destinations.



One key difference is that the old westbound Arboretum off-ramp has been removed, and its function replaced by a new off-ramp now open at 24th Avenue East.

We worked closely with the city of Seattle and local communities to integrate neighborhood traffic management strategies and support anticipated traffic patterns before, during and after construction.

Environmental and community benefits of the WABN project

The project provides a number of design features and mitigation projects that will provide direct benefits to the local community and environment.

Environmental mitigation

Aquatic, wetland and parks mitigation measures will result in:

- A new Arboretum trail.
- A new public park just north of SR 520 along Portage Bay.
- Removal of the westbound Arboretum off-ramp and unused R.H. Thomson “Ramps to Nowhere.”



A view of the old westbound off-ramp to Lake Washington Boulevard being removed from the Arboretum.

Bicycle and pedestrian connectivity

A new regional shared-use bike path will connect Seattle and the Eastside, and improve bicycle and pedestrian connectivity to local trails and transit stops.



A new bike/ped path will extend from the floating bridge to an exit at 24th Avenue East in Montlake, giving bicyclists and pedestrians a new route between Seattle and the Eastside.

Transit connectivity

Extending a dedicated transit/HOV lane to Montlake will maintain existing bus service and access, and improve transit connectivity and reliability.



WSDOT, Sound Transit, King County Metro and other agencies worked together to build the Montlake Multimodal Center, which provides connections to light rail and buses, and provides a safe bicycle and pedestrian path over Montlake Boulevard.

Sustainability

In addition to the improvements for bicyclists, pedestrians and transit riders, design refinements reduced WABN’s concrete volumes by nearly 40 percent compared to the original design.

Limiting construction impacts

As we proceed with construction, we are committed to construction management practices that avoid, minimize and mitigate the effects of our construction activities on neighbors, communities, and the traveling public.

Our construction best practices include:

- Providing the public with advance notice of highway closures and lane closures on local streets.
- Limiting highway closures to nights and weekends to keep weekday traffic flowing.
- Keeping Montlake Boulevard and other local streets open during daytime construction hours, and limiting lane closures of local streets to nighttime.
- Keeping a navigation channel on Lake Washington open for boaters.